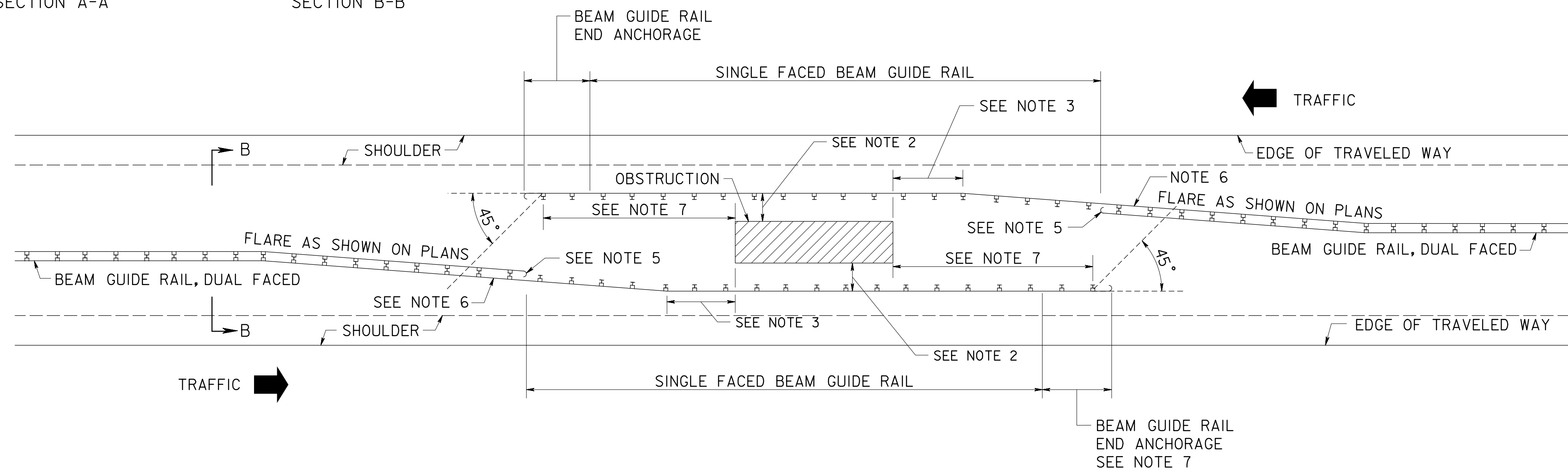
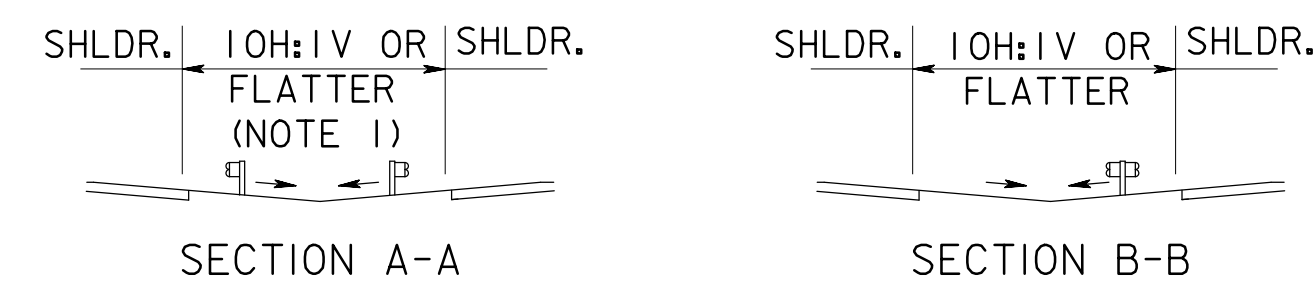


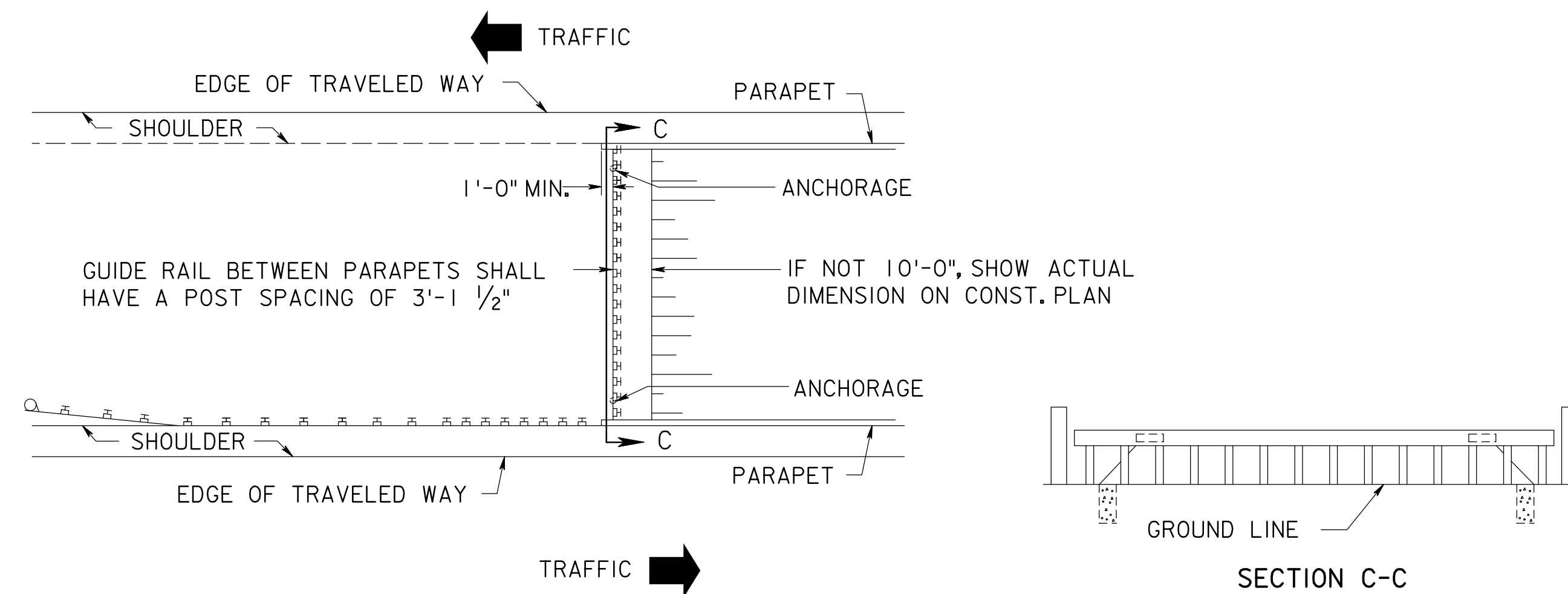
MEDIAN GUIDE RAIL WITH IMPACT ATTENUATOR 2-WAY TRAFFIC

N.T.S.



OVERLAPPING DUAL FACED MEDIAN BEAM GUIDE RAIL 2-WAY TRAFFIC

N.T.S.



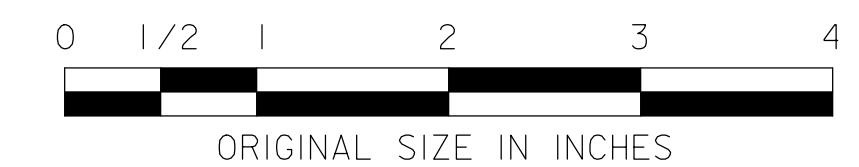
MEDIAN GUIDE RAIL TREATMENT AT ADJACENT BRIDGES

N.T.S.

NOTES:

1. 10V:1V OR FLATTER SLOPES TO BEGIN 100' IN ADVANCE OF THE IMPACT ATTENUATOR.
2. MINIMUM CLEARANCE FROM THE FRONT FACE OF W-BEAM GUIDE RAIL ELEMENT TO THE OBSTRUCTION SHALL BE 4'-9". WHERE MINIMUM CLEARANCE CANNOT BE ACHIEVED, SEE GR-10.
3. A MINIMUM OF 12'-6" TANGENT SPACE IS REQUIRED BETWEEN THE FLARE AND THE OBSTRUCTION.
4. IMPACT ATTENUATOR TO BE INSTALLED AS PER THE MANUFACTURER.
5. ATTACH AN END SECTION (ROUNDED) WHERE DUAL-FACED BEAM GUIDE RAIL TERMINATES AND SINGLE FACE GUIDE RAIL CONTINUES. SEE GR-1.
6. EXTEND DUAL FACED GUIDE RAIL A MINIMUM OF ONE 6'-3" SPACE (TWO POSTS) BEYOND A 45 DEGREE LINE EXTENDED FROM THE LAST POST OF THE BEAM GUIDE RAIL ANCHORAGE.
7. SEE GR-6 FOR GRADING AND RECOVERY AREA DETAILS ASSOCIATED WITH BEAM GUIDE RAIL END ANCHORAGE.

MASH TL-3



NEW JERSEY TURNPIKE AUTHORITY
**NEW JERSEY TURNPIKE
 GARDEN STATE PARKWAY**
 STANDARD DRAWINGS

MEDIAN GUIDE RAIL TREATMENTS - 1

OFFICE OF THE CHIEF ENGINEER
 NEW JERSEY TURNPIKE AUTHORITY
 WOODBRIDGE, NEW JERSEY

STANDARD DRAWING

REV.	DESCRIPTION	DATE
0	ORIGINAL DRAWING	07/21

GR-7