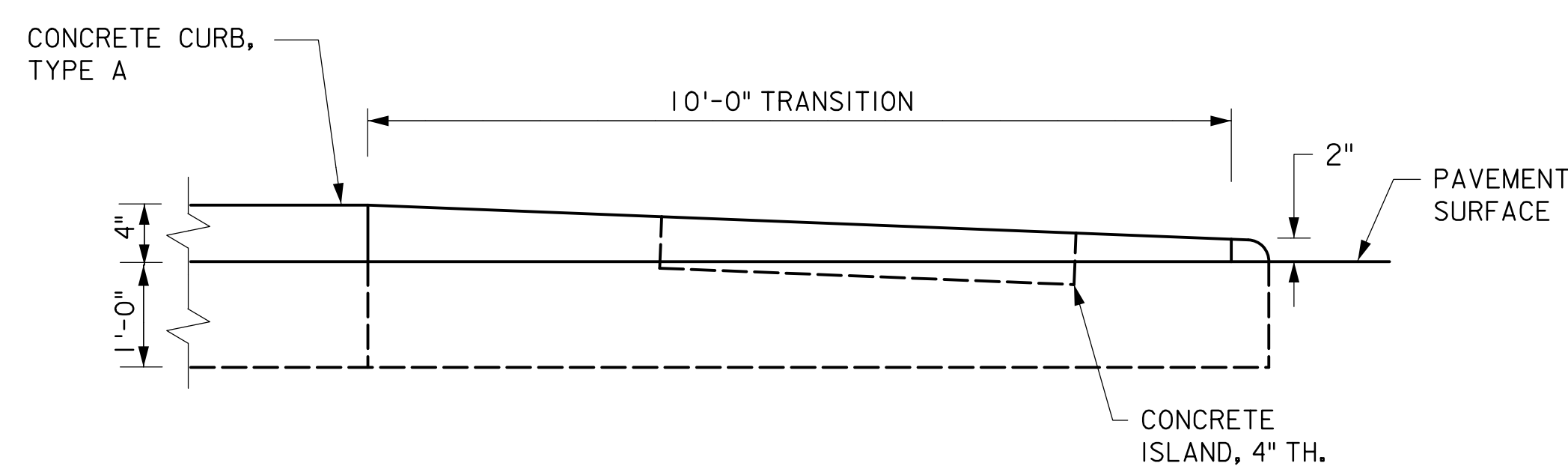
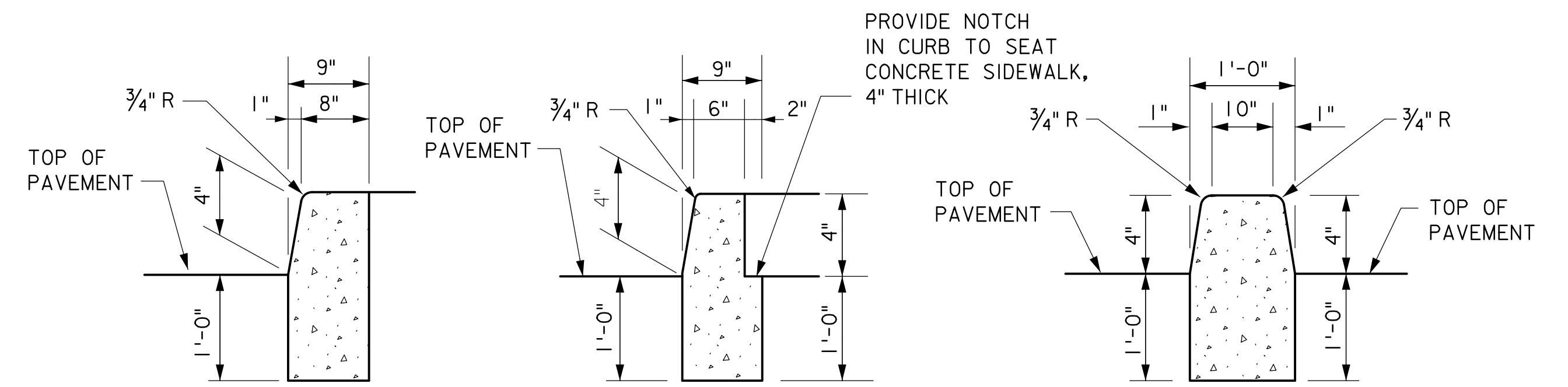


SECTION A-A



SECTION B-B

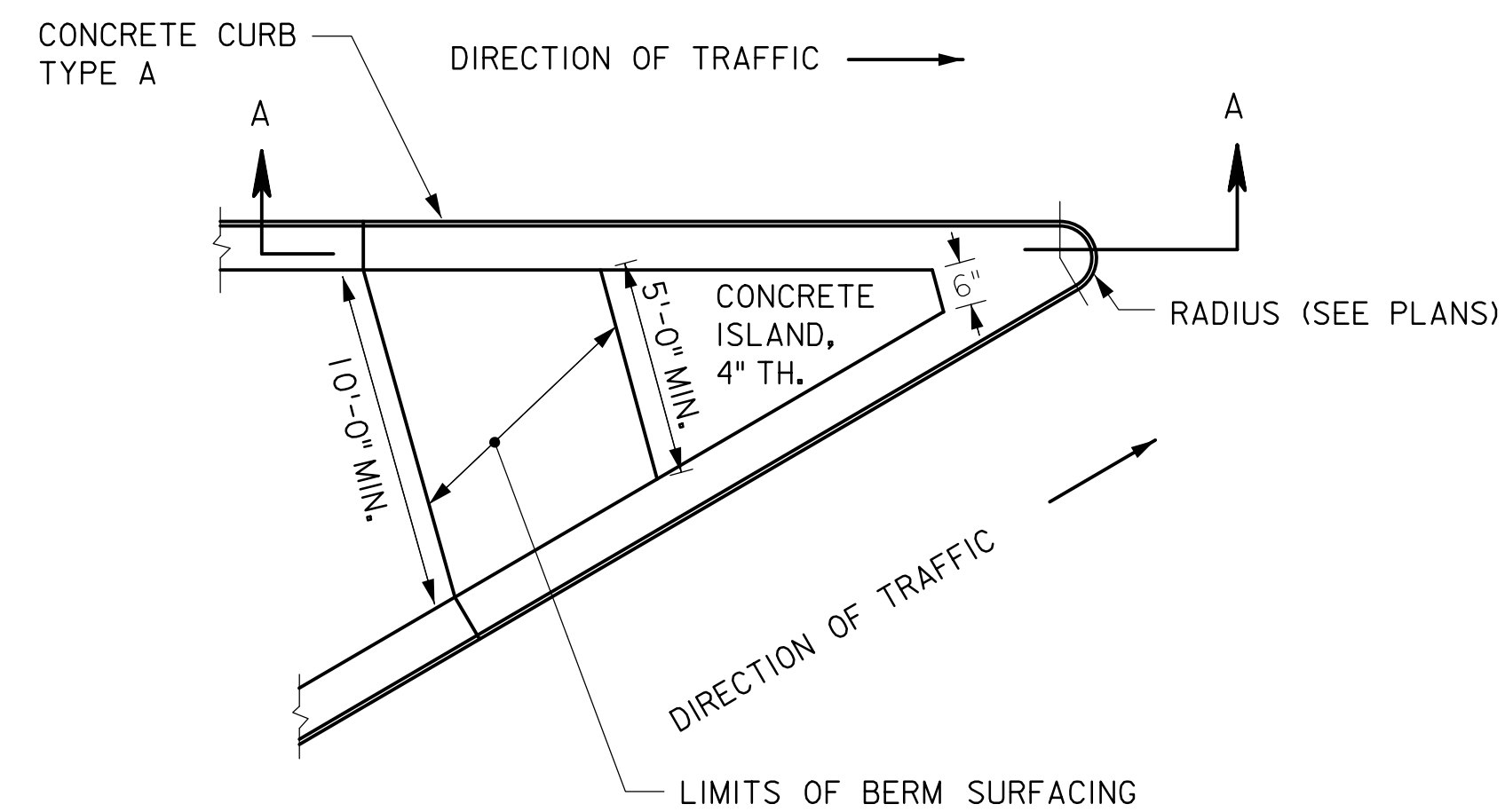


TYPE A

TYPE B

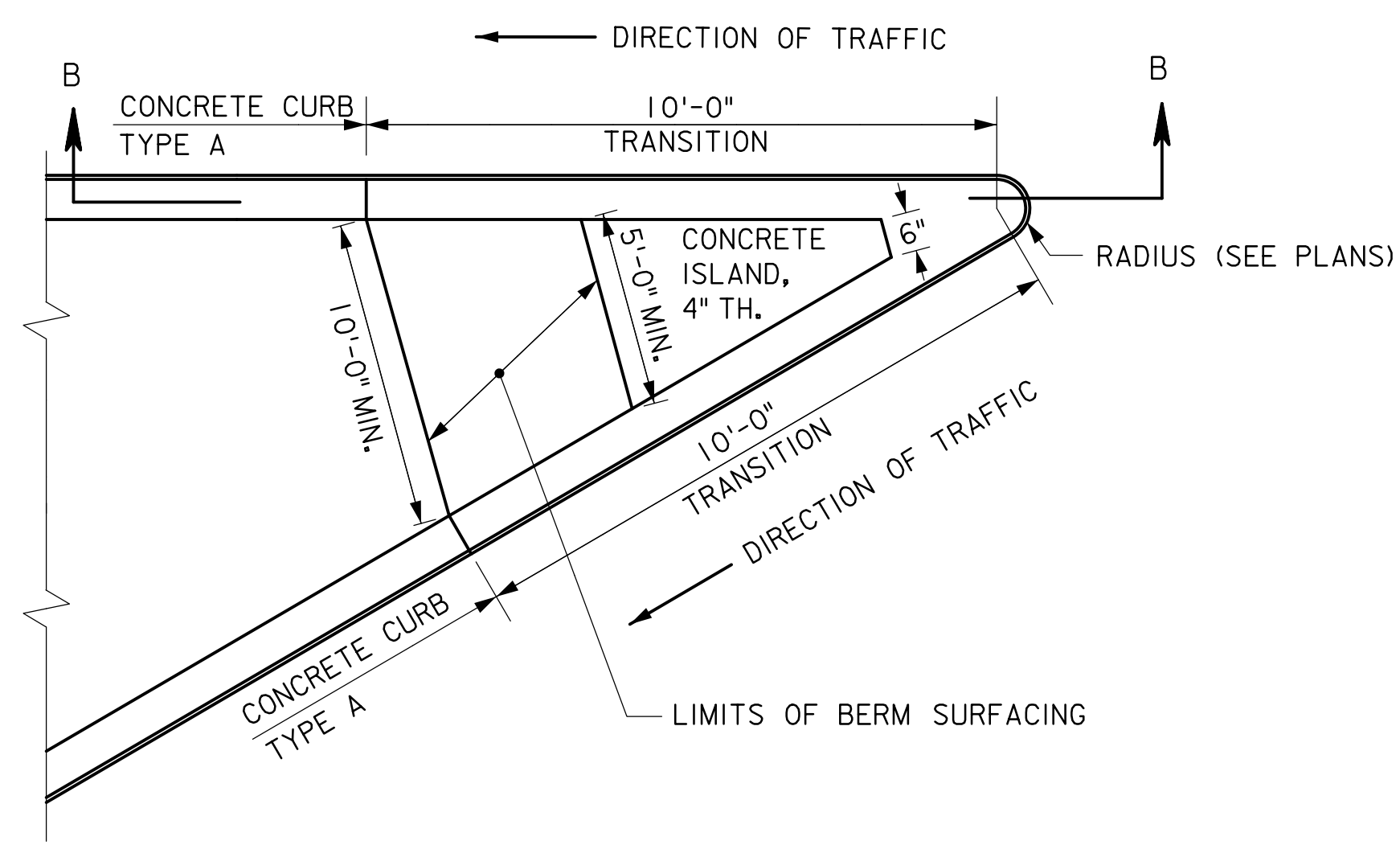
TYPE C

CONCRETE CURB DETAILS



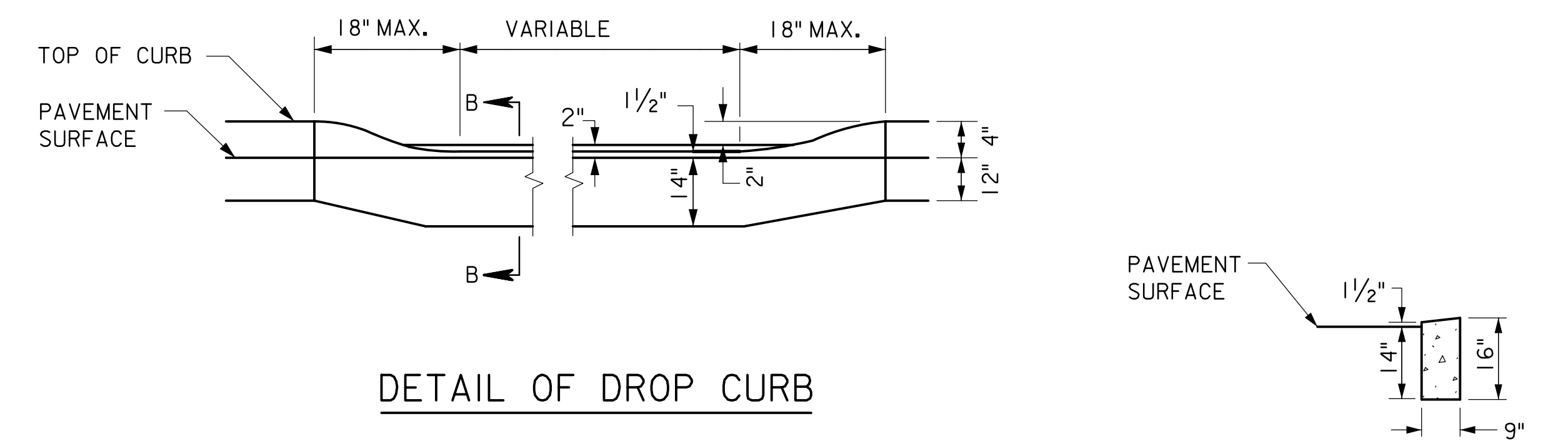
PLAN

TYPICAL CURBED NOSE- MERGING TRAFFIC



PLAN

TYPICAL CURBED NOSE- DIVERGING TRAFFIC

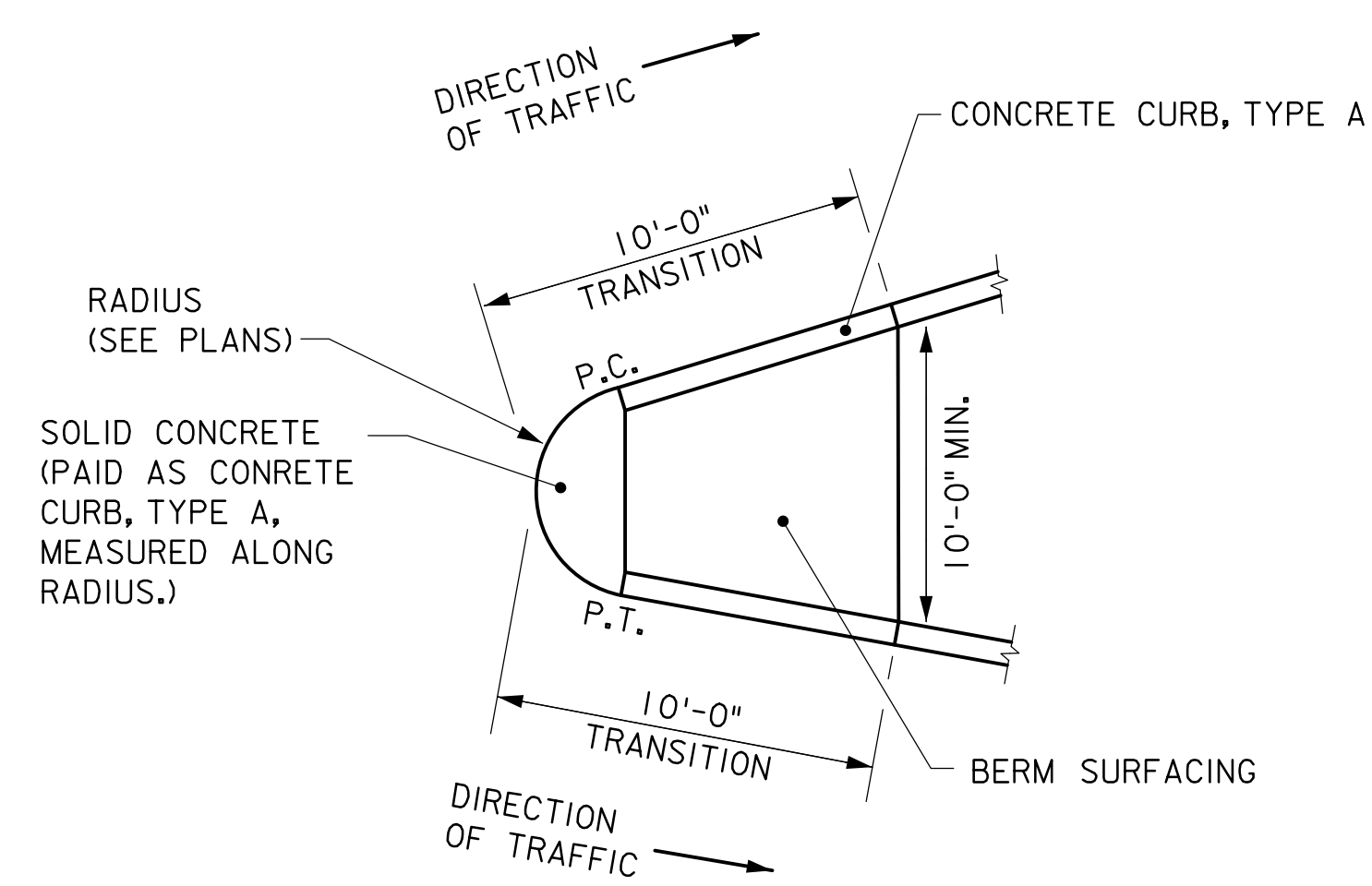
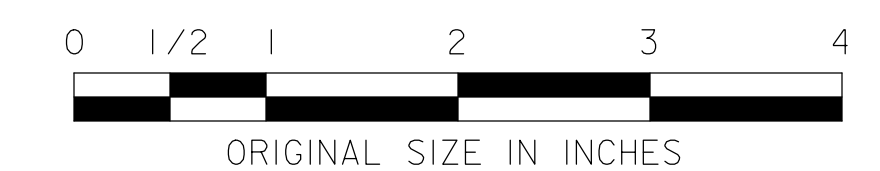


DETAIL OF DROP CURB

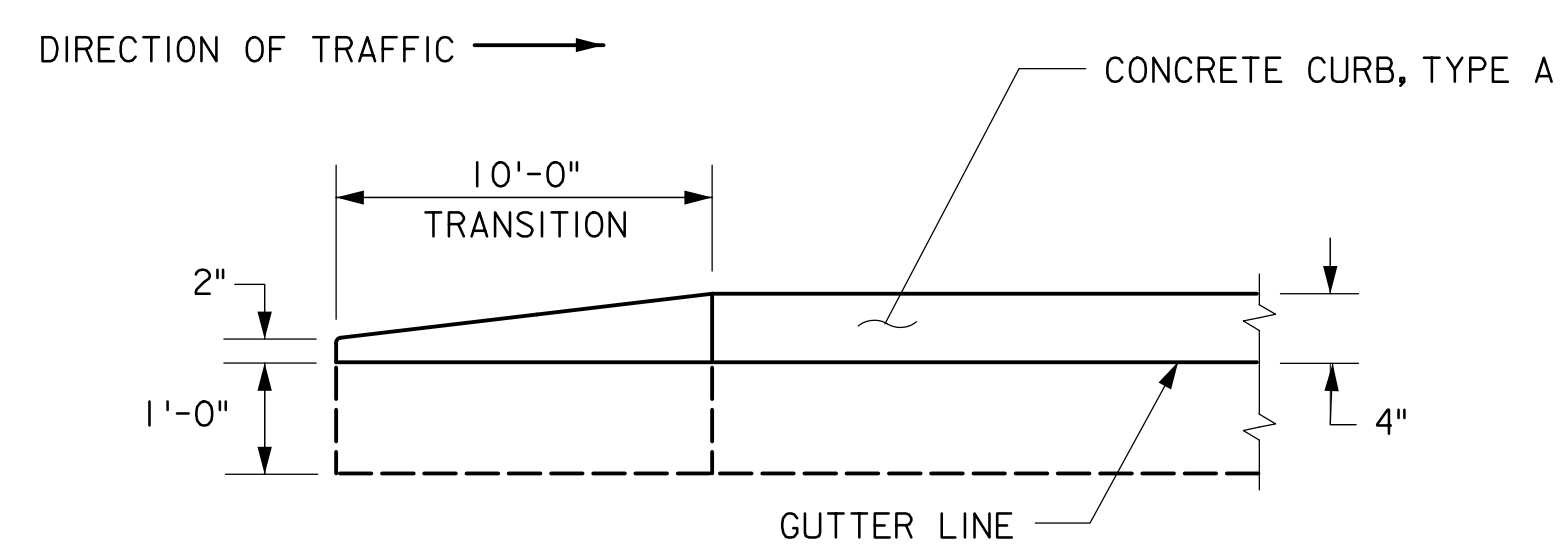
SECTION B-B

NOTES:

1. ALL CONCRETE CURB AND NOSE SHALL BE CLASS "A," AIR ENTRAINED.
2. EXPANSION JOINTS SHALL BE PROVIDED A MAXIMUM OF 20' O.C. IN NOSE AND CURBS.
3. CONCRETE NOSE SHALL HAVE CONTRACTION JOINTS A MAXIMUM OF 5' O.C.



ALTERNATE CURBED NOSE - DIVERGING TRAFFIC



CURB TREATMENT, APPROACHING TRAFFIC

REV.	DESCRIPTION	DATE
0	ORIGINAL DRAWING	07/21

NEW JERSEY TURNPIKE AUTHORITY  
**NEW JERSEY TURNPIKE  
 GARDEN STATE PARKWAY**  
 STANDARD DRAWINGS

**CONCRETE CURB AND NOSE DETAILS**

OFFICE OF THE CHIEF ENGINEER  
 NEW JERSEY TURNPIKE AUTHORITY  
 WOODBRIDGE, NEW JERSEY

STANDARD DRAWING  
**CU-1**