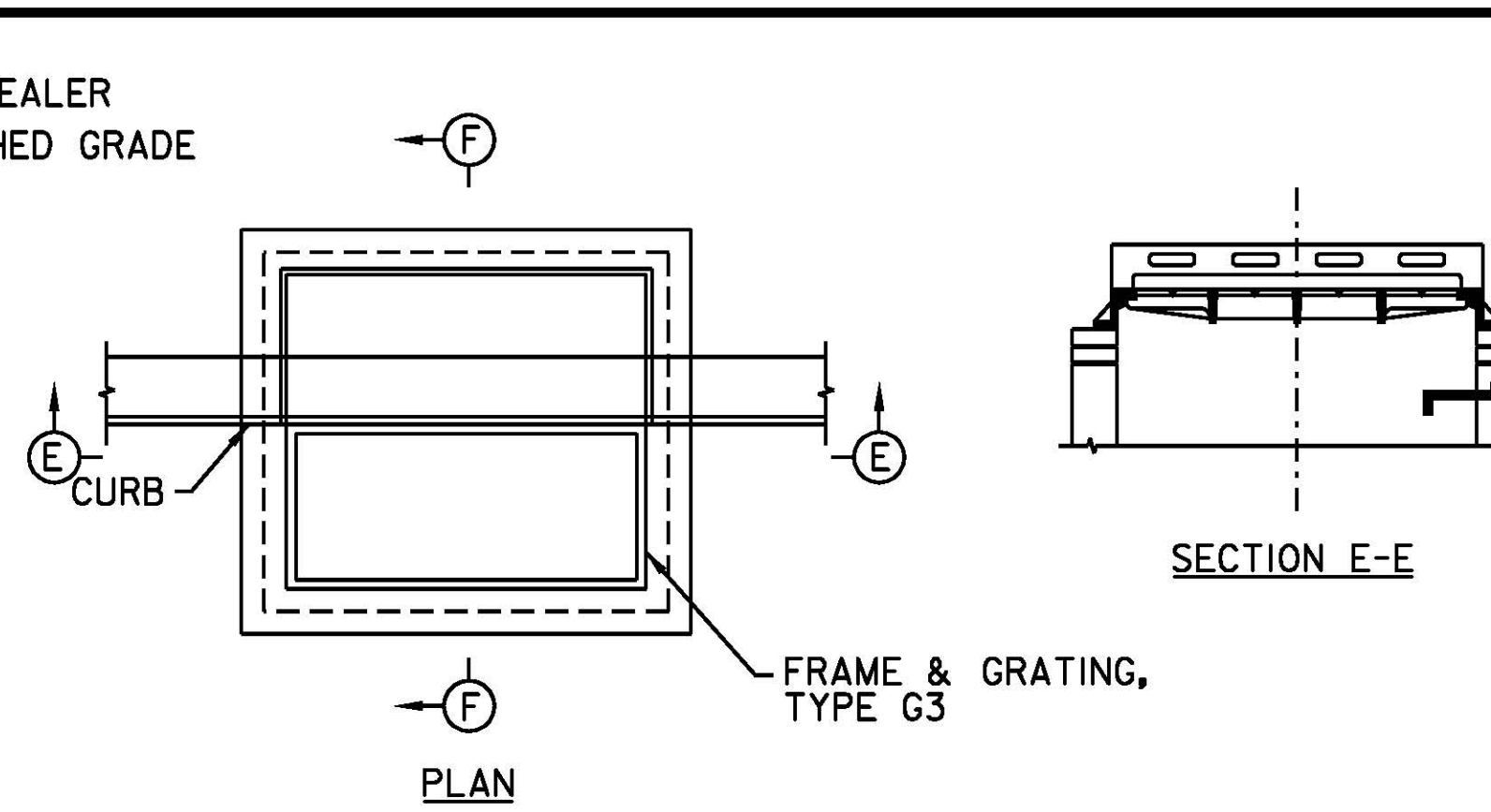


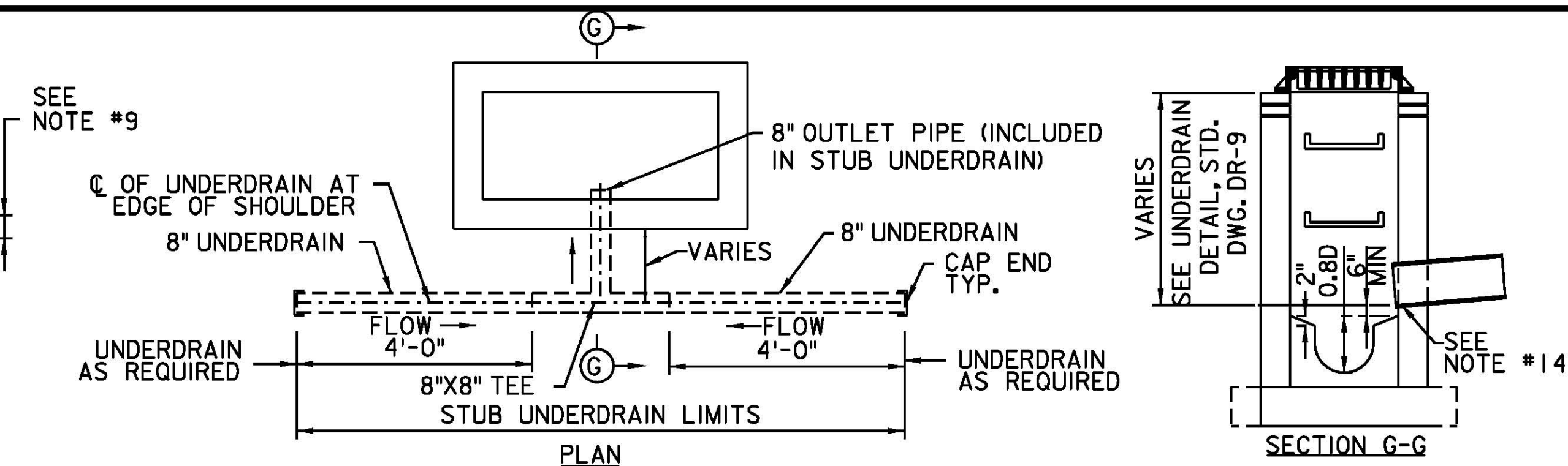
**MEDIAN INLET PLACEMENT AT TWO WAY RAMPS**

- NOTE:**
- A. SEE INLET, TYPE D-1 FOR DIMENSIONS AND DETAILS.
  - B. HOT POURED JOINT SEALER AND EQUALIZER PIPE TO BE INCLUDED IN THE COST OF D-1 INLET.



**DROP INLET D3**

- NOTE:**  
SEE DROP INLET, TYPE D2 FOR PIPE & INLET DETAILS.



**STUB UNDERDRAIN AT DROP INLET**  
(SEE NOTE 5)

- NOTES:**
1. WALLS OF DROP INLETS SHALL BE 6" THICK & BE CONSTRUCTED OF EITHER (A) CLASS A CONCRETE WITH AIR ENTRAINMENT, CAST IN PLACE OR PRECAST. OR (B) SOLID CONCRETE BLOCK.
  2. BASES OF DROP INLETS TO BE SITE CONSTRUCTED OF CLASS "D" CONCRETE WITH AIR ENTRAINMENT; OR PRECAST-CLASS A, WITH AIR ENTRAINMENT.
  3. LADDER RUNGS TO BE PROVIDED IN ALL DROP INLETS OVER 4' DEEP.
  4. PIPES MAY ENTER OR LEAVE DROP INLETS IN ANY DIRECTION, AS INDICATED ON THE PLANS.
  5. STUB UNDERDRAIN TO BE PROVIDED AT EVERY INLET AND TO BE PAID FOR AS 10 LIN. FT. OF 8" PIPE UNDERDRAIN EXCEPT WHERE A REGULAR UNDERDRAIN SYSTEM IS USED.
  6. ALL INTERIOR JOINTS TO BE POINTED FULL AND FLUSH.
  7. WHERE DEPTH OF RECTANGULAR CONCRETE BLOCK DROP INLETS EXCEED 10 FEET, WALL THICKNESS BELOW THE 8 FEET DEPTH SHALL BE 1'-0". MAXIMUM DEPTH OF CONCRETE BLOCK DROP INLETS TO BE 13 FEET.
  8. FOR FRAME AND GRATING DETAILS, SEE STANDARD DRAWING DR-2.
  9. ADJUST TO GRADE WITH CONCRETE BLOCK OR BRICK AS REQUIRED, (12" MAX.), WHEN USING BRICK.
  10. BOTTOM SECTION OF PRECAST INLET WALLS TO BE A MAXIMUM OF 8'-0" FROM OUTLET PIPE INVERT TO TOP OF JOINT.
  11. FRAMES TO BE SET IN FULL BED OF STIFF MORTAR. (SEE STD. DWG. DR-2)
  12. FOR LADDER RUNG DETAILS, SEE STD. DWG. DR-3.
  13. INLET CHANNEL CAN BE FORMED CONCRETE OR A COMPOSITE OF POURED CONCRETE AND CONCRETE BLOCK.
  14. INVERT OF UNDERDRAIN PIPE NOT TO BE LOWER THAN THE TOP (INSIDE DIAMETER) OF THE OUTLET PIPE.
  15. MINIMUM WALL REINFORCEMENT FOR PRECAST INLETS:

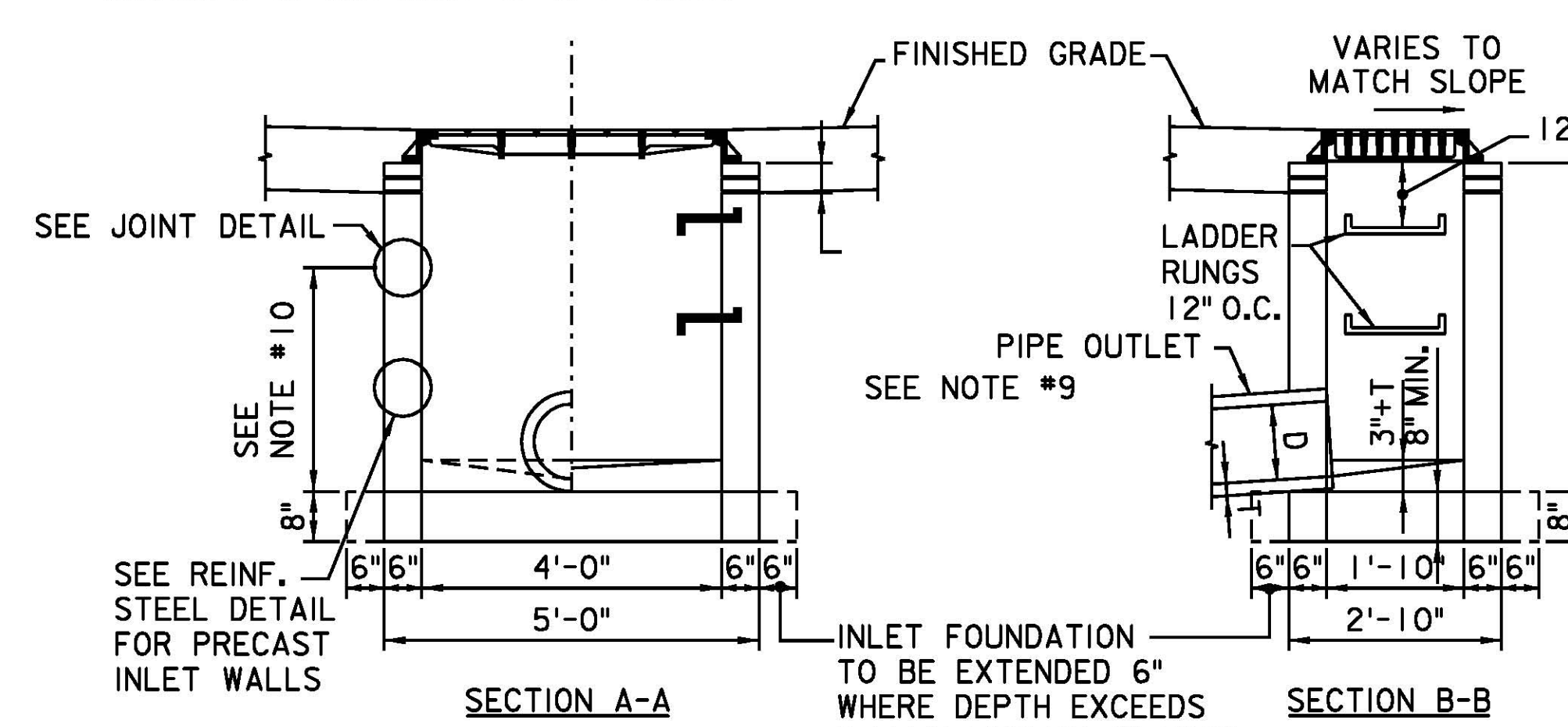
DEPTH BELOW TOP OF GRATE	BAR REINFORCEMENT		
	HORIZONTAL REINF.	VERTICAL REINF.	WALL THK.
0' TO 10'-0"	#4 @ 10" C.C.	#4 @ 18" C.C.	6"
10'-1" TO 15'-0"	#4 @ 8" C.C.	#4 @ 18" C.C.	6"
15'-1" TO 20'-0"	#4 @ 6" C.C.	#4 @ 18" C.C.	6"

REINFORCING SHOWN FOR PRECAST INLETS IS THE MINIMUM REQUIRED. ADDITIONAL REINFORCING FOR HANDLING IS THE RESPONSIBILITY OF THE CONTRACTOR

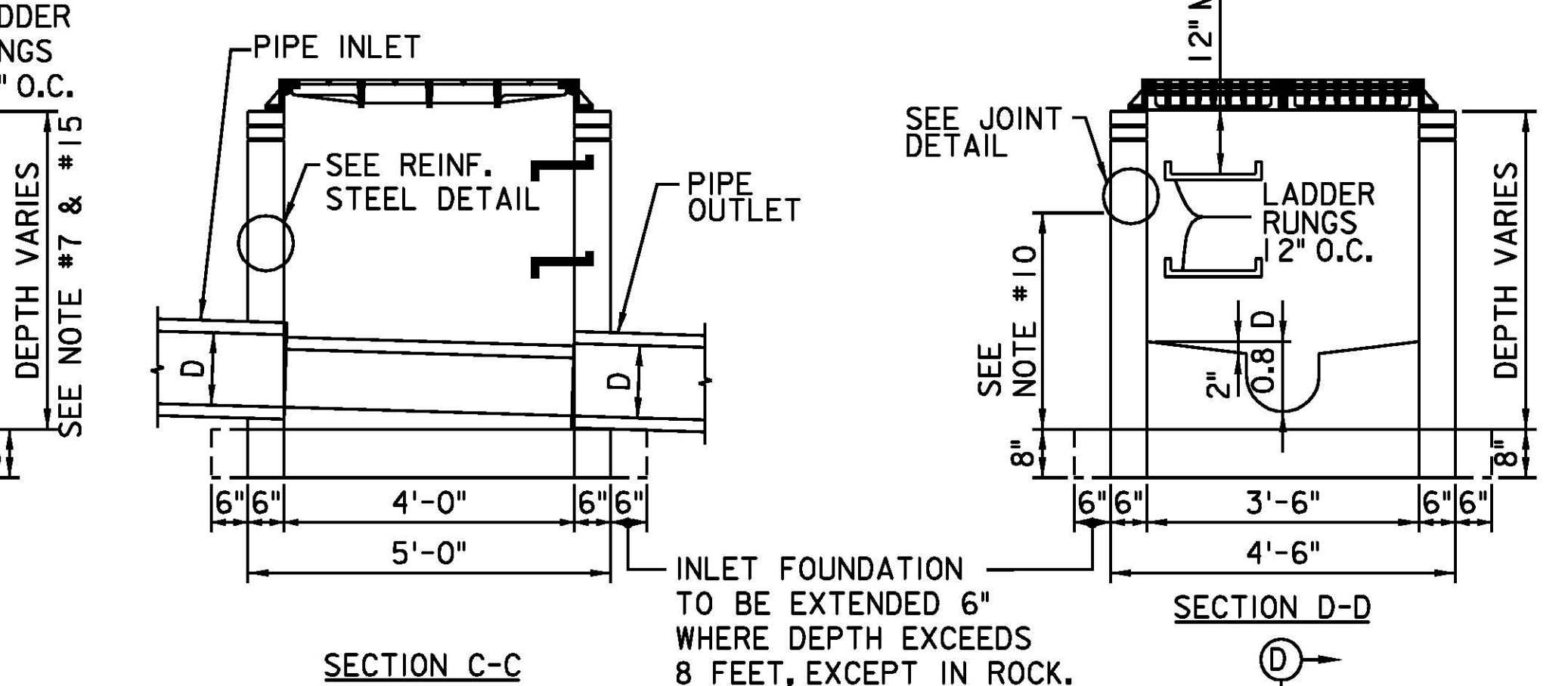
**WWF REINFORCEMENT**

0' TO 10'-0"	WWF 3 X 6 W6 WIRES SPACED AT 3" TO RUN HORIZONTAL IN ALL CASES.
10'-1" TO 15'-0"	WWF 3 X 6 W6 ADD #3 BAR @ 18" HORIZONTAL
15'-1" TO 20'-0"	WWF 3 X 6 W6 ADD #3 BAR @ 9" HORIZONTAL OR ADD #4 BAR @ 15" HORIZONTAL

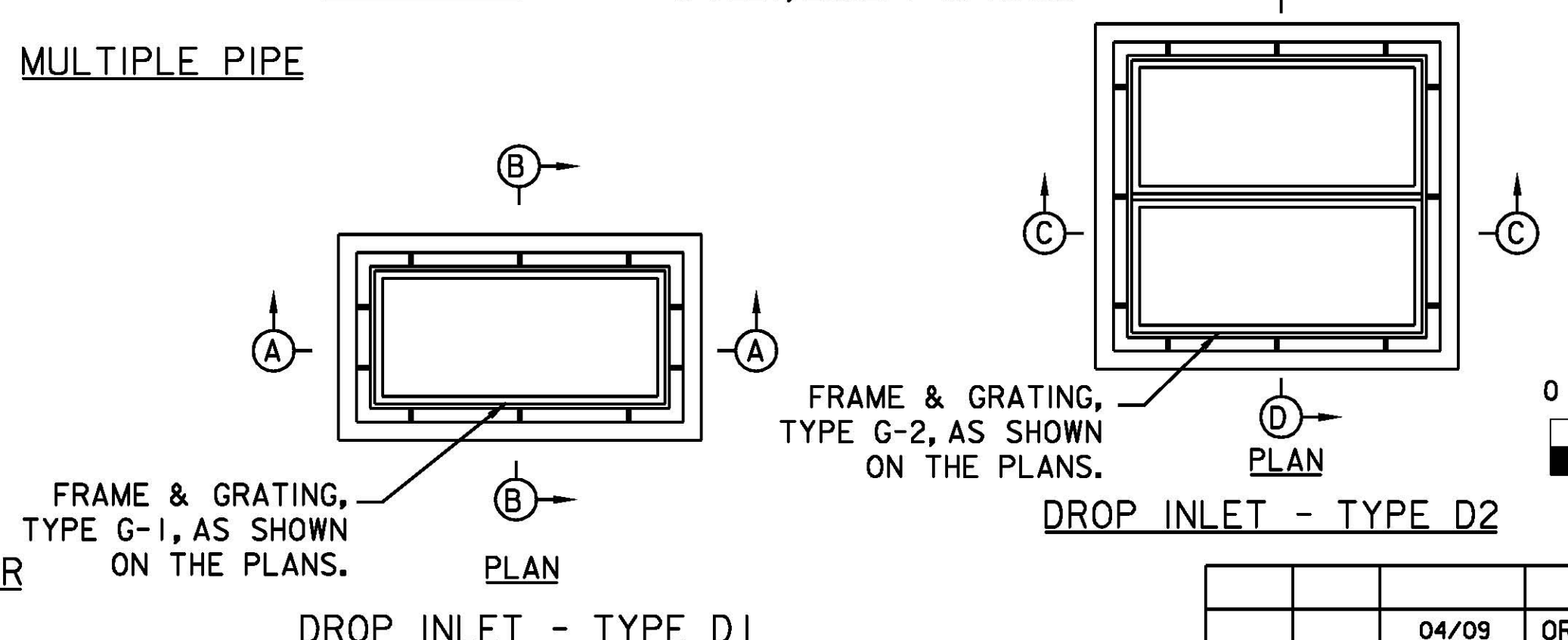
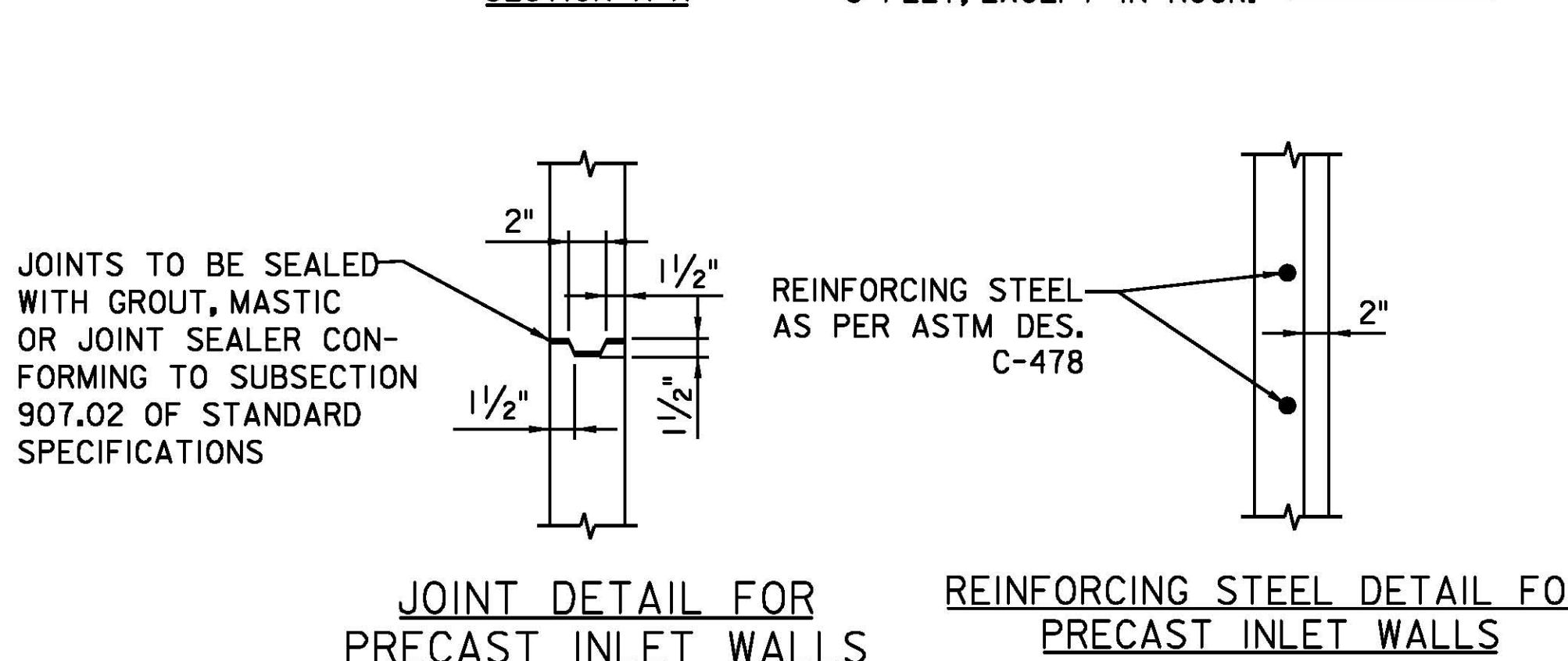
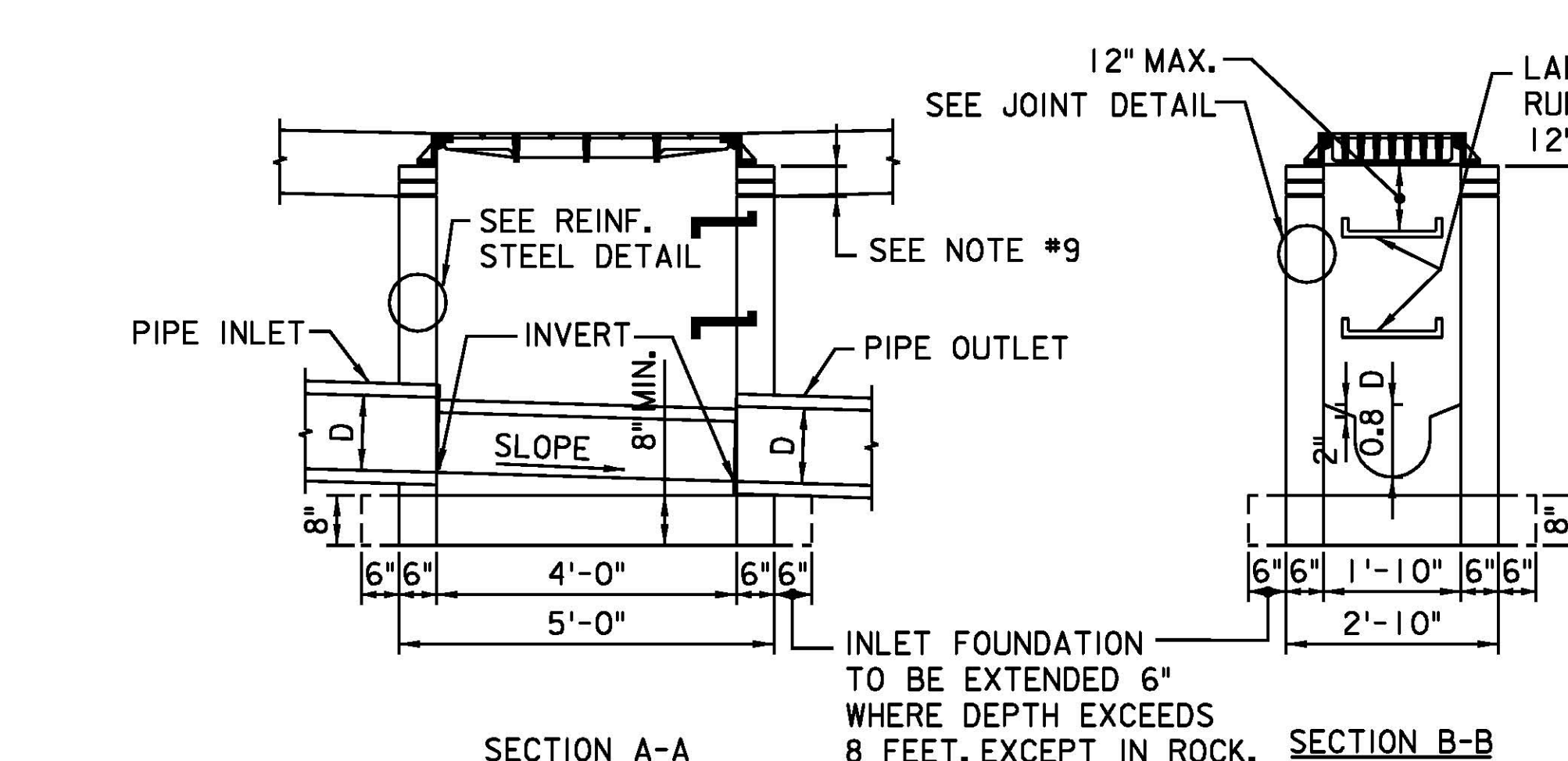
16. NO INLETS OR MANHOLES SHALL BE LOCATED IN THE TRAVEL LANES. WHEN LOCATED IN SHOULDERS, ONLY SNOW INLETS AT LOW POINTS SHALL BE PLACED.
17. GUTTER INLETS SHALL BE RECESSED 3 FEET FROM THE GUTTERLINE-EDGE OF PAVEMENT MEASURED FROM THE CENTERLINE OF THE GRATE.



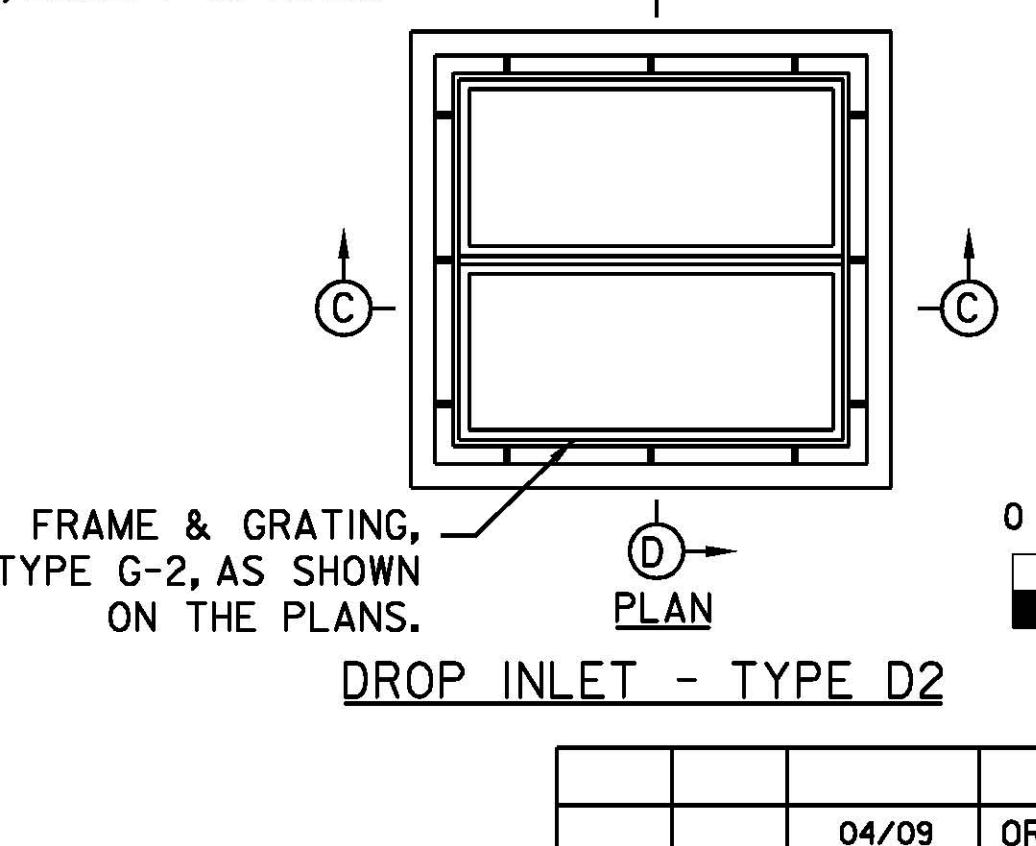
**SINGLE PIPE**



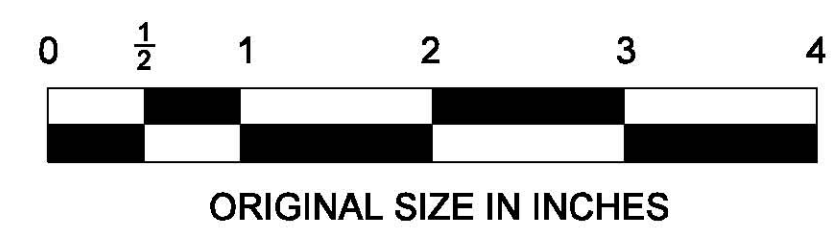
**MULTIPLE PIPE**



**DROP INLET - TYPE D1**



**DROP INLET - TYPE D2**



ORIGINAL SIZE IN INCHES

App.	No.	DATE	REVISION
		04/09	ORIGINAL DRAWING

CONTRACT NO.

NEW JERSEY TURNPIKE AUTHORITY

**DROP INLETS DETAILS**

OFFICE OF THE CHIEF ENGINEER

2009 STANDARD DRAWING

DR-1

SHEET NO.

OF