

New Jersey Turnpike Authority

PO Box 5042, Woodbridge, NJ 07095



Document Change Announcement

2007 Design Manual

DCA2014-DM-02

DATE: March 11, 2014

Subject: Revisions to Sections 1.A.5. and 1.B.6 regarding Traffic Control

Description of Change

This change revises submittal requirements pertaining to traffic control on local roadways that require NJDOT review and involvement in addition to local county and/or municipality resolutions establishing installation or modification of a traffic control device or establishing a traffic regulation. Traffic control is specific to local road detours exceeding 48 hours and traffic control affecting State highways. Sections affected include 1.A.5. and 1.B.6.

Instructions to Designers and Consultants

Effective immediately, the revisions contained in this announcement shall be applied to all projects that have not reached Phase C of design. Contact your NJTA Project Manager for instructions. Attached revision is noted in italics.

Designers may access these revisions in the NJTA Design Manual, which is available on the Authority's Web Page: <http://www.state.nj.us/turnpike/professional-services.html>.

Information for In-House Staff

The revisions have been incorporated into the Design Manual, which is available on the S drive @ S:\Project Files\Design-Procedure Manual. Please distribute the information to your respective Project Managers and have them direct their consultants appropriately.

Approved By:

A handwritten signature in black ink, appearing to read "R. Fischer", is written over a horizontal line.

Robert J. Fischer, P.E.
Chief Engineer

New Jersey Turnpike Authority

DOCUMENT UPDATE REQUEST

Forward to Chief Engineer

Initiator	Ana Lucia Tatoris	Submittal Date	2/11/2014
Firm	NJTA	Telephone	X8262

Date 2/11/2014

Document (check one)

- Procedures Manual
- Design Manual
- Sample Plans
- Standard Drawings
- Standard Specifications

Description of Change

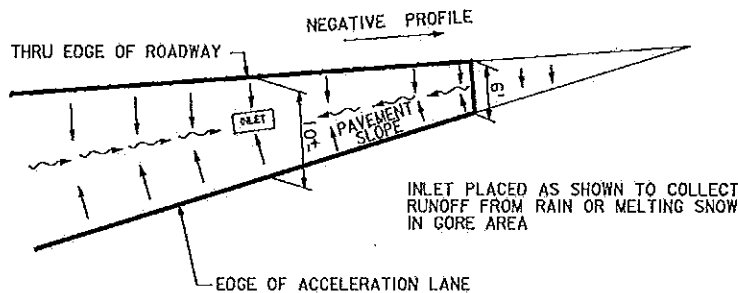
This change revises submittal requirements pertaining to traffic control on local roadways that require NJDOT review and involvement in addition to local county and/or municipality resolutions establishing installation or modification of a traffic control device or establishing a traffic regulation. Traffic control is specific to local road detours exceeding 48 hours and traffic control affecting State highways. Sections affected include 1.A.5. and 1.B.6.

Reason for Change

The Re-adoption with Amendments of N.J.A.C. 16:27 in 2013 define the procedure to obtain approval for traffic control on local roadways that require NJDOT review and involvement in addition to local county and/or municipality resolutions/ordinances. They include roadway closures (detours) of non-State highways not under Authority jurisdiction in excess of 48 hours and for traffic control devices on a local road that impacts State highways. These changes incorporate the new procedures.

4. Typical nose grading between mainline roadway and auxiliary lanes is shown in Exhibit 1A - 39.

EXHIBIT 1A - 39
TYPICAL NOSE GRADING



1A.4.3 Climbing Lanes

With a maximum of 3 percent grades, the Authority does not use truck climbing lanes. As indicated in Subsection 1A.2.6, the absolute minimum length of vertical tangent shall be limited by a maximum permissible loss in truck speed of 10 mph.

1A.5 OTHER ROADWAYS

1A.5.1 Crossroads

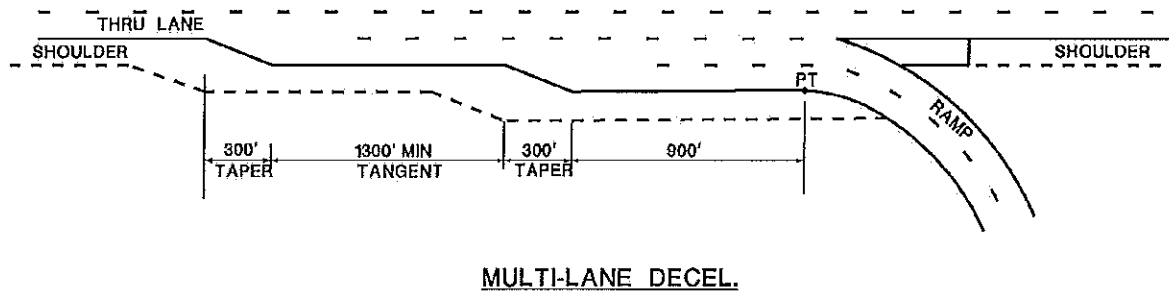
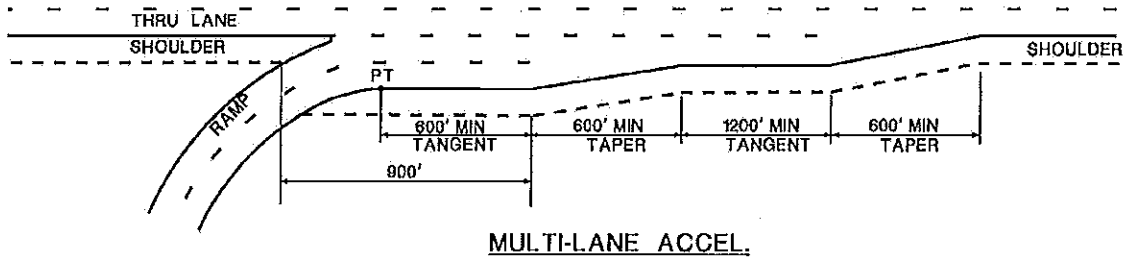
Where local roads are being replaced, the intent of the Authority with respect to any work under the jurisdiction of the state, county, municipality, or any other agency is "replacement in kind", according to the present standards of that agency. All such work is subject to the approval of the Authority's Engineering Department and must be previously agreed to in writing by the concerned agency, as noted elsewhere in this manual and the Procedures Manual.

Similarly, all detouring and/or closing of local roads during construction must be previously approved in writing by the concerned appropriate agencies in accordance with the Procedures Manual.

1A.5.2 Access and Service Roads

1. Treatment shall be similar to Subsection 1A.5.1.
2. For parking lots and driveways at toll plaza buildings and other locations within the Turnpike right of way, the pavement section shall be as shown on Exhibit 1A - 40 Exhibit 1A—40. Refer to Subsection 1A.2.7 for additional information and details.

**EXHIBIT 1B - 33
RAMP TERMINAL TREATMENT MULTI-LANE RAMP**



1B.6 OTHER ROADWAYS

1B.6.1 Crossroads

Where local roads are being replaced, the intent of the Authority with respect to any work under the jurisdiction of the state, county, municipality, or any other agency is "replacement in kind", according to present standards of that agency. All such work is subject to the approval of the Authority's Engineering Department and must be previously agreed to in writing by the concerned agency, as noted elsewhere in this manual and the Procedures Manual.

Similarly, all detouring and / or closing of local roads during construction must be previously approved in writing by the appropriate concerned agencies in accordance with the Procedures Manual.

1B.6.2 Access and Service Roads

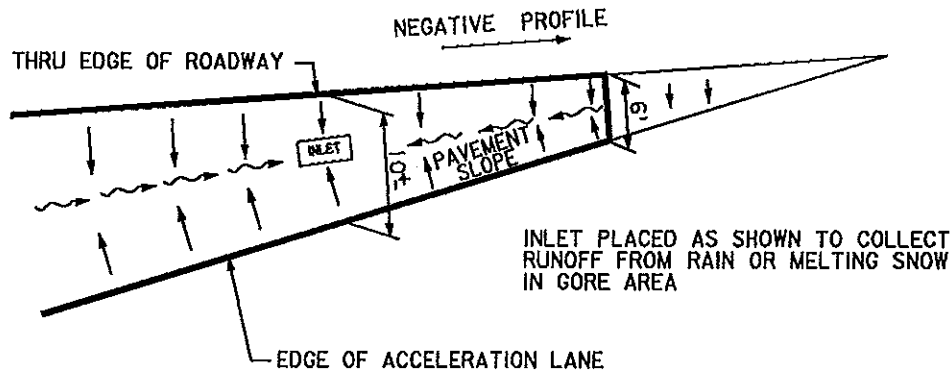
Treatment similar to "Crossroads" above.

1B.6.3 U-Turns and Z-Turns

1. U-Turns and Z-Turns shall be designated by milepost location. Refer to Section 6B for signing.
2. Use and Location
Consult NJTA for location criteria.

4. Typical nose grading between mainline roadway and auxiliary lanes is shown in Exhibit 1A - 39.

EXHIBIT 1A - 39 TYPICAL NOSE GRADING



1A.4.3 Climbing Lanes

With a maximum of 3 percent grades, the Authority does not use truck climbing lanes. As indicated in Subsection 1A.2.6, the absolute minimum length of vertical tangent shall be limited by a maximum permissible loss in truck speed of 10 mph.

1A.5 OTHER ROADWAYS

1A.5.1 Crossroads

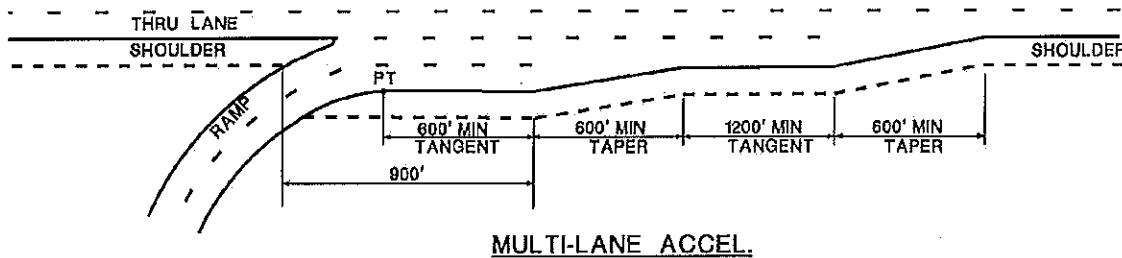
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Similarly, all detouring and/or closing of local roads during construction must be **approved by the appropriate agencies in accordance with the Procedures Manual**.

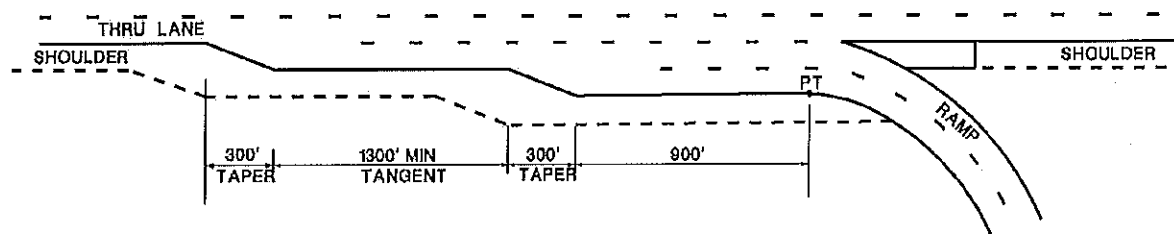
1A.5.2 Access and Service Roads

1. Treatment shall be similar to Subsection 1A.5.1.
2. For parking lots and driveways at toll plaza buildings and other locations within the Turnpike right of way, the pavement section shall be as shown on Exhibit 1A - 40. Refer to Subsection 1A.2.7 for additional information and details.

EXHIBIT 1B - 33 RAMP TERMINAL TREATMENT MULTI-LANE RAMP



MULTI-LANE ACCEL.



MULTI-LANE DECEL.

1B.6 OTHER ROADWAYS

1B.6.1 Crossroads

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Similarly, all detouring and / or closing of local roads during construction must be *approved by the appropriate agencies in accordance with the Procedures Manual*.

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